

# APPENDIX B: EXISTING PROGRAMS AND DOCUMENTS

## EXISTING PROGRAMS SUMMARY

In addition to the natural and built environment, the social environment in the form of bicycling programs and resources helps to create, engage, and sustain a bicycle friendly community. A useful framework for describing the categories into which such resources fall is the five E's: Engineering, Education, Encouragement, Enforcement, and Evaluation. While the first E represents physical infrastructure (described in depth in the master plan document), the last four include primarily programmatic elements. A review of the existing programmatic bicycle environment in Greeley gives a baseline for the planning effort to build on and learn from. The following programs and resources currently exist in the City of Greeley for Greeley area residents. Although a number of programs (noted within) have been very successful, most programs have not been developed to their full potential due to lack of staffing available to coordinate and perpetuate the programs.

### Education

Education programs teach bicyclists of all ages how to ride safely on the road, including bicycling skills, bicycle-related laws, and how to interact with other modes of traffic.

#### *Bike Rodeos*

In Spring of 2013, the city's traffic engineering department began hosting bicycle rodeos to teach children key bike handling skills and rules of the road through a series of drills and obstacle courses. Bicycle rodeos have been held at the Poudre River Trail-athon for the past few years.

#### *Adult Education*

The city has hosted educational classes in the past to teach bicycle safety to adults when instructors were available. Until recently, there were no League of American Bicyclist Certified Instructors (LCIs) in Greeley to regularly host the skills trainings, so Greeley encouraged interested residents to attend seminars in nearby Fort Collins. However, a city parks employee was recently certified, and the city intends to start hosting educational classes again in Greeley.



#### *Public Service Announcements*

In the past, the City of Greeley has developed Public Service Announcements to explain recent bicycle projects and infrastructure, such as the signage promoting Greeley Bikes and the Greeley Bikes website. Currently, the city has a potential funding source under review to develop radio spots to educate the public on bike safety, such as sharing the road, biking to school, and rules of the road.

### *Share the Road Training*

As part of new employee training, Greeley trains city staff, transit operators, and school bus operators on how to share the road with bicyclists.

### **Encouragement**

Encouragement programs motivate new riders to try bicycling. From marketing campaigns to group rides, encouragement programs aim to improve potential and existing bicyclists' confidence in their ability to use bicycling as a transportation and recreation option and to increase the perception that bicycling is a safe and efficient activity. Events and programs in the city of Greeley do not have a centralized person, department, or organization through which they are coordinated, making advertising, scheduling, and organizing significantly more burdensome for the individuals who do coordinate events.

### *Bicycle Friendly Community*

In May 2013, the League of American Bicyclists identified the City of Greeley as a bronze level Bicycle Friendly Community. The designation recognizes Greeley's efforts to improve conditions for bicycling through investing in bicycle infrastructure, programs and campaigns, and bicycle-friendly policies.



### *Bicycle Clubs/Shops*

A number of bicycle clubs and shops organize rides and provide information and coordination for the Greeley bicycle community. Bicycle clubs include The Two Rivers Cycling Club which assisted the city with the Bicycle Friendly Community application and organizes rides including the Fahrenheit Classic and the Oktoberfest Ride, the UNC Cycling Club which is comprised of full time UNC students, and the Greeley Triathlon Club. Area bike shops include Bike Peddler, Ride Greeley, International Bike, Greeley Bike and Scooter, and George's Repair Shop. Turn Around Bikes is a bike shop and community organization that builds and donates bicycles to those in need of transportation and hosts educational maintenance and encouragement workshops.

### *Bicycle Resource Website*

The Greeley Transportation Department hosts a website, [www.GreeleyBikes.com](http://www.GreeleyBikes.com), which is a dedicated bike page on the city's website. The Greeley Bikes page provides basic information about bicycling, including the following:

- Details about upcoming events, including community rides
- Educational information, including commuter tips, rules of the road, and how to ride in the winter
- Links to bicycle and trail maps
- Descriptions and/or videos of recent bicycle projects (public service announcements)
- City contacts

In 2103, the city began installing plaques advertising the website on posts below bike lane signs. The plaques are meant to inform the residents about the website as a means of disseminating safety information to current and future riders and encouraging residents to give bicycling a try.

The city also maintains the Greeley Bike Blog, [www.greeleygov.com/greeleybikes/BikeBlog.aspx](http://www.greeleygov.com/greeleybikes/BikeBlog.aspx), which highlights upcoming events, such as Bike Month activities, and provides safety tips and advice.



### *Community Group Rides*

There are several community bike rides that the City of Greeley supports through in-kind funding and helps organize throughout the year. Family Bike Nights occur on the first Tuesday of every month from May through September. Other rides are focused around holidays and city events, and include Critical Mass, the Zombie Zoom, and the Moonlight Ride (hosted by the Greeley Police Department). The Moonlight Ride has been very successful throughout its 18 years as an event, with as many as 250 participants. Family rides are becoming more well attended (approximately 15 to 40 riders), with additional longer rides being added throughout the summer and requests for more. The city advertises all community rides on the Greeley Bikes website.

### *National Bike Month*

The City of Greeley promotes Bike Month in June every year through community rides, a series of activities and accompanying published guides outlining the events, and valet bike parking at events.

The city hosts a well-attended Bike to Work Day at the end of June as part of Bike Month. This event, previously organized by city staff, has been coordinated by staff from the North Colorado Medical Center for the past few years. The day includes breakfast stations for people who ride to work, prizes, and promotional items, such as t-shirts. The Bike to Work Day Facebook page provides tips about commuting and reminders about the event's activities leading up to Bike to Work Day.



### *SmartTrips of Northern Colorado*

SmartTrips is a web-based program that is intended to help residents of northern Colorado travel as often as possible by means other than driving alone in a car, including carpooling, walking, telecommuting, taking the bus, and bicycling. The program creates a personalized model to fit each participant's personal circumstances and work, and is available for use by area residents on their own initiative.

### *Green Government Bikes*

The City of Greeley has recently purchased a fleet of 20 "Green Bikes" available for loan to city employees. The program, part of the city wellness program, started in 2014 and has seen regular participation throughout the summer. Bicycles are available at various city buildings, including City Hall and Annex, the court building, A Street Complex, and Union Colony Civic Center.

### *Bicycle Advocacy Group*

The Greeley Bicycle Advocacy Group was developed 1.5 years ago to promote bicycling within the region, serve as a clearing house for bicycle events and causes, and review and provide guidance on bicycle projects and policies. This advocacy group is a community-based organization that meets once a month and is comprised of members from the planning, transportation, and recreation departments, city manager's office, law enforcement, Weld County staff, regional health organizations, Greeley bicycle organizations, and area residents. This organization is involved in aspects of many of the area's bicycle and non-bicycle events including coordination of events such as a recent bicycle-themed movie night with bike parking provided by the advocacy group. Since its formation, it has developed into a consistent resource for the Greeley bicycle community and plays a significant "behind the scenes" role in promoting bicycling in Greeley.

## Enforcement

Enforcement programs, typically spearheaded by law enforcement agencies, improve the safety of bicyclists by enforcing laws and behaviors that contribute to comfortable bicycling environments.

### *Police Officer Training*

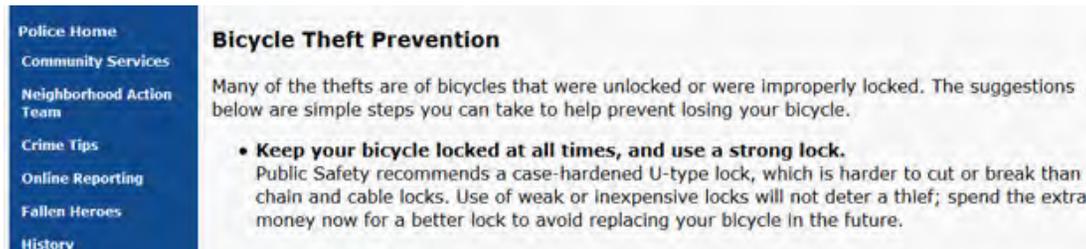
The Greeley Police Department officers go through a bicycle component as part of their basic academy training so that they are familiar with how the law applies to bicycling.

### *Targeted Enforcement*

To improve the bicycling environment in Greeley, the Police Department conducts targeted enforcement of bicyclist and motorist activities that lead to unsafe conditions. Targeted enforcement can include motorists not yielding to bicyclists, motorists passing bicyclists too closely, bicyclists riding without lights, or bicyclists riding against traffic.

### *Bicycle Theft Services*

On their website, the Greeley Police Department includes tips on how to prevent bicycle theft, such as proper locking techniques, as well as basic bike maintenance and operation safety tips. The department also provides a free, non-mandatory bicycle registration program that helps in recovering stolen bicycles and serves as a deterrent to theft.



**Bicycle Theft Prevention**

Many of the thefts are of bicycles that were unlocked or were improperly locked. The suggestions below are simple steps you can take to help prevent losing your bicycle.

- **Keep your bicycle locked at all times, and use a strong lock.** Public Safety recommends a case-hardened U-type lock, which is harder to cut or break than chain and cable locks. Use of weak or inexpensive locks will not deter a thief; spend the extra money now for a better lock to avoid replacing your bicycle in the future.

### *Bicycle Registration and Licensing*

The Greeley Police Department provides a voluntary registration service where residents can submit information about themselves and their bicycle and be entered into a database of registered bicycles. This program is called both “licensing” and “registration” on city forms, and is effectively a registration program, providing the police department a database to look up bicycles that are reported stolen. City staff have noted that this program appears to act as a deterrent to residents who prefer not to be in a database, as they are unsure of whether they or their children are legally allowed to ride a bicycle in Greeley without registering their bicycle or obtaining a license as a bicycle rider. This program is not highly utilized, and the question of which department should manage the program is currently under discussion.

### **Evaluation**

Evaluation programs measure and evaluate the impact of projects, policies and programs. Typical evaluation programs range from a simple year-after-year comparison of US Census Journey to Work data to regular bicycle counts and community surveys. The City of Greeley doesn’t currently have any evaluation programs or procedures in place, and as a result does not have any before and after comparisons of bicycle programs and facilities implemented to date.

### *Bicycle Counts*

Bicycle counts and community surveys act as methods to evaluate not only the impacts of specific bicycle improvement projects but can also function as a way to measure progress towards reaching a city’s sustainability goals. In December 2010, Greeley purchased its first infrared camera for counting bicyclists and pedestrians and has since measured bicycle volumes at a number of trailhead locations. See the master plan document for count information.

### *Bicycle Parking*

The city recently completed a bicycle parking inventory to use as a baseline in determining bicycle parking capacity in relation to potential demand and for comparison in the future as bicycle support facilities improve. See the master plan document for inventory information.

## EXISTING PLANS AND DOCUMENTS SUMMARY

Plans and documents prepared by local and regional agencies provide a background on current and past goals, efforts, and plans for bicycling in the city and region as well as a framework for future planning and development. Review of these plans and documents serves as a way of identifying potential future project partners and providing background support for future grants applications. A decade of regional and local planning and policy documents were reviewed as part of this planning effort. Very few of the efforts are directly related to bicycle planning. Yet, all levels of land use, transportation, and urban design must be considered together as it's their coordination, or lack thereof, that ultimately determines the appeal of bicycling for recreation, transportation, and utility purposes. These existing plans and documents were reviewed to determine applicability to the bicycle master planning process and to gather data collected as part of previous projects. As applicable, they will also inform the master planning process where the project team feels that they align with the vision and goals of the current master planning effort.

## Bicycle Plans

### North Front Range Regional Bike Plan

Agency: North Front Range Metropolitan Planning Organization (MPO)

Date Published: 2013

This Regional Bicycle Plan provides a summary of existing conditions for bicycling throughout the region, and identifies opportunities for improvement including Regional Bicycle Corridors. It identifies 12 Regional Bicycle Corridors in

the Northern Front Range MPO - three of which traverse through Greeley: the Poudre River Trail along the north side of Greeley, the Greeley LaSalle Corridor running north-south through the heart of Greeley, and the US 34 Corridor. Implementation phasing or timeframe was not discussed due to the regional coordination nature of the plan.



### North Front Range Bike Plan Survey

Agency: North Front Range MPO

Date Published: 2012

This survey, completed prior to the Regional Bicycle Plan, assesses resident behaviors related to bicycle use, barriers to riding a bicycle at all and to riding more often, and resident opinion related to concerns about bicycling in the region. It discusses priorities for a regional bicycle system and the locations of destinations that should be included in the North Front Range Regional Bike Plan. It also provides survey results and discusses findings including the following: Greeley non-riders would be more inclined to ride their bikes if there were more on-road facilities such as bike lanes or if street/road conditions were better, such as smooth pavement and less debris.

### League of American Bicyclists Feedback Report

Agency: League of American Bicyclists

Date Published: 2013

This Feedback Report was prepared by the League of American Bicyclists (LAB) as a response to the Greeley application to the LAB Bicycle Friendly Community program. It provides key performance measures and additional recommendations for improving bicycling in Greeley; ideas are categorized by low hanging fruit and fast results and long term goals. It also summarizes benefits of improving bicycling in Greeley. Recommendations identified as key measures for Greeley are summarized below:

- Bicycle Advisory Committee should meet monthly
- Expand and focus the time spent by the person coordinating Bicycle and Pedestrian efforts for the city
- Reduce traffic speeds, including consideration of lowering the speed limit to 25 mph or lower on non-arterial roads
- Provide designated bicycle facilities and calm traffic speeds along arterial and collector roadways

- Host a League Cycling Instructor (LCI) seminar to increase the number of certified LCIs in Greeley
- Include bicycle safety education as a routine part of public education, and increase safety and convenience of bicycling to schools and in the surrounding neighborhoods
- Expand encouragement efforts during Bike Month in partnership with advocacy groups
- Encourage area institutions of higher education to promote bicycling and apply for Bicycle Friendly Community status



## **Open Space and Trails Plans** *Parks, Trails, and Open Lands (PTOL)* *Plan*

Agency: City of Greeley

Date Published: Adopted August 20, 2013

This plan was developed by the City of Greeley to plan for parks, trails, and open lands with a goal of maintaining the existing level of service of these facilities as the population grows. Specific mentions of existing or proposed facilities related to bicycles are listed below:

- Facilities Inventory - Describes and inventories existing “connective spaces”
- Facility Needs Assessment - Assesses the need for 26.32 miles of additional trails by 2030. “City staff will need to identify an appropriate level of service and build new facilities or expand existing facilities accordingly”
- Connective Spaces - Includes policies and objectives to carry out goal 3 of the PTOL Plan “Preserve, develop and/or maintain a network of trails, greenways, travel corridors, entryways, and view sheds that connect neighborhoods with parks, open lands, recreation and other facilities, and open community separators that help Greeley to maintain its unique identity.”

Distinguishes between developed trails and undeveloped greenways and discusses trail spurs as opportunities for connections.

- Implementation - Identifies top PTOL priorities for the 2013-2018 timeframe and identifies potential resources and funding tools. Identifies further study of alignment for Poudre Trail east of 11th Avenue as a prioritized activity.

### *Greeley Parks and Trails Master Plan*

Agency: City of Greeley

Date Published: 2002

This document is a contributing document to the PTOL plan described above, completed approximately ten years earlier. Major bicycle related facilities, needs, and recommendations discussed in this document are listed below:

- Existing Resources - provides a general description of the existing multi-purpose trail network including the Poudre River Trail
- Issues & Needs - describes recreational trends in Colorado, and nationally, in support of more trails throughout Greeley. Briefly discusses the opportunities and challenges associated with constructing trails and identifies a major need is to connect

the large student population at UNC with the downtown, and the central city population with the rest of the city trail system

- Recommendations - Provides recommendations for design standards of trails that are primarily recreational in nature, versus bike transportation corridors that are part of the higher speed, on-street bike lane system. Recommends acquisition of the right-of-way and construction of 68 miles of new trails throughout the community and Urban Growth Area (UGA) with the intent to locate new trails in open space corridors, away from arterial road systems wherever possible. Trails along the Poudre River and Sheep Draw to US34 are identified as a high priority

### City of Greeley Conceptual Trails Plan

Agency: City of Greeley

Date Published: 2002

This plan, which was also a contributing document to the PTOL plan described above, provides conceptual trail alignments for regional trails with the intent to guide the city's decision process as development and infrastructure improvements arise. The concepts presented do not show trails paralleling roads unless they are necessary to complete the regional system. Other local on- and off-street connecting trails may be necessary to complete the trail system.

### Transportation Plans

#### 2035 Comprehensive Transportation Plan

Agency: City of Greeley

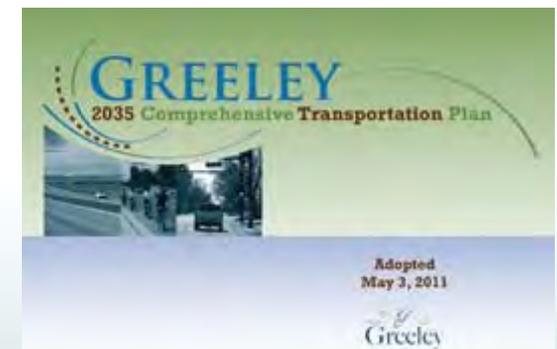
Date Published: 2011

The Greeley 2035 Comprehensive Transportation Plan "defines the long-range vision for the multi-modal transportation system that offers choices in how people travel." This plan reviews the relationship between transportation needs and land use, and is one of the first transportation plans completed by Greeley to emphasize the development and perpetuation of a balanced transportation system that offers choices in how people travel. As a result, it discusses bicycle facilities in a number of ways, summarized below:

- Existing Transportation Conditions - Describes existing bike network by trails, shared use paths, bike lanes, and bike routes. Discusses different users and suitable facilities for accommodating users
- Vision Plan – The bicycling vision developed as part of this plan is "The development of a comprehensive bicycle network that serves all population groups."
- Functional Classification Criteria

Characteristics – defines and describes bike lanes and shared use paths as appropriate for major arterials, minor arterials, and collectors, and lays out a general description of lane configuration for each classification type

- Bicycle Vision Plan - Includes a needs description as well as proposed facility improvements. Also incorporates maintenance, 5 Es, minimum bike parking requirements, and multimodal connectivity
- Plan Implementation – Identifies "policies" (action steps) to achieve plans goals as they relate to bicycling. Pertinent action steps include: Identify critical bike maintenance and improvement projects as part of CIP, review and update bike plan every three years, provide a continuous bike route system, reserve or acquire ROW for future bikeways, and evaluate potential to retrofit the street system to incorporated bike facilities



### *North Front Range MPO Regional Transportation Plan*

Agency: North Front Range MPO

Date Published: 2011

This plan provides regional 2010-2015 Goal Statements, Implementation Strategies, and Specific Actions for regional agencies. As these relate to bicycle planning, it identifies the need to develop a Regional Bicycle and Pedestrian Plan to minimize traffic congestion. In addition, it identifies Six Livability Principles including 1. Provide more transportation choices - Develop safe, reliable and economical transportation choices to decrease household transportation costs, reduce our nation's dependence on foreign oil, improve air quality, reduce greenhouse gas emissions and promote public health.

### *2040 Statewide Transportation Plan (in progress)*

Agency: CDOT

Date Published: Anticipated 2015

Plan may include regional/statewide priorities for bicycle infrastructure/routes/crossings, etc. in Greeley.

### *Greeley Evans Transit Transfer Center Design and Route Planning (in progress)*

Agency: City of Greeley

Date Published: In progress

The City of Greeley is in the process of designing a new transfer center for the Greeley Evans Transit (GET) system to replace the existing downtown transfer center, which is located on 7th Street between 10th and 9th Avenue and is currently a shelter-type facility only. The existing transfer center is planned to be removed and a new one installed on A Street and 14th Avenue. In addition, a route planning study is underway and is intended to examine the operation of the existing routes and make modifications based on the new transfer center location and the city's changing transit needs.

### **Comprehensive/Land Use Plans**

#### *Greeley Comprehensive Plan - 2060: Ideally Greeley*

Agency: City of Greeley

Date Published: 2011

The 2060 Greeley Comprehensive Plan builds on the Comprehensive Plan adopted in 2000 to provide a planning framework and policy guide for future Greeley development and improvement. As such, it discusses transportation elements and bicycle-related policies specifically as summarized below:

- Introduction & Perspective - Provides a brief context for the bicycle infrastructure in Greeley; highlights the role of the Bicycle Vision Plan that is part of the 2020 Transportation Plan in identifying needed facilities including shared use paths and bicycle lanes; amenities to enhance connections to other travel modes, and striping standards. All collector and arterial streets are designed to have on-street bike lanes. Additionally discusses abandoned railroad lines as good locations for future bicycle and pedestrian trails
- Provides transportation-related goals, policies, objectives & actions:

- Promote pedestrian and bicycle travel in school routing plans; enforce traffic regulations which favor and protect pedestrians and bicyclists; provide detached bicycle paths on major collectors and all arterials streets to separate non-commuter bicyclists from high-speed, high-volume traffic wherever possible; use standards for on-street bike lane widths which incorporate practical use and multi-modal expectations; develop standards with input from local commuter bicyclists and other users
- Formulate Alternative Compliance Credits for parking where it is possible to incorporate practical and realistic pedestrian, transit, bicycle and other alternative travel into the design of a development to reduce on-site parking needs; establish minimum off-street requirements for bicycles; revenue saved from reducing street width should be devoted to bicycle lanes
- Review and revise, as feasible, design standards for intersections in a manner which prioritizes pedestrian and bicycle comfort and safety
- Adopt and implement target level-of-service standards for all components of the transportation system including bicycle - strive for a Service Level of 'B' for pedestrian and bicycle movements throughout the city

Table 8 :: Greeley Public Works General Street Design Parameters

Street Classification	Arterial, Multi-modal			Local			Local Residential	Local Low-Volume
	4-6 Lanes	Major Arterial 4 Lanes	Minor Arterial 4 Lanes	Major Collector 4 Lanes	Minor Collector 2 Lanes	Commercial 2 Lanes		
Right-of-Way Width	140'/165'	125'/135'	110'	100'	75'	50'	60'	50'
Roadway Width*	94'	78'	78'	73'	50'	37'	34'	24'
Number of Lanes	4-6	4	4	4	2	2	2	2
Travel Lane Width	12'	12'	12'	11'	11'	11'	10'	10'
Bike Lanes	Y	Y	Y	Y	Y	N	N	N
Bike Lane Width	10'***	6'	6'	6'	6'	0	0	0
Parking Lane Width	None	None	None	None	8'	None	7'	None
Traffic Volume ADT	35,000	20,000	15,000	10,000	3,500	5,000	1,500	500
Posted Speed MPH	55	50	45	40	35	30	30	30

\* Flow Line to Flow Line

\*\* Detached Bikeway in lieu of on-street Bike Lane

Note: Design parameters taken from existing design criteria.

## Policy and Design

### City of Greeley 2014 Street Maintenance Program

Agency: City of Greeley

Date Published: 2014

A Maintenance Plan is developed and updated by Greeley Public Works staff every year to determine roadway maintenance procedures and priorities. This plan includes both regular ongoing maintenance and also roadways scheduled for repaving or more major maintenance, which provides an opportunity to identify short-term bike facility improvements with roadways already planned for modifications.

## City of Greeley Street Design Criteria and Construction Specifications

Agency: City of Greeley

Date Published: 2008

The City's Street Design Criteria and Construction Specifications discuss bicycle facilities in a number of locations. Primarily they are shown in a number of Standard Roadway Section Details, some of which call out a bicycle component as standard, some of which are optional. Not all bicycle facility types are defined and guidance given.

## *University of Northern Colorado Bicycle Regulations*

Agency: University of Northern Colorado (UNC)

UNC has a voluntary bicycle registration program for anyone wishing to park or ride their bicycle on campus which costs \$20 and includes a U-lock. Bicycles with any current city registration or license may re-register with Parking Services at a reduced rate of \$15. Bicycles locked to anything besides bike racks including trees, hand railings, garbage cans, etc., are subject to fines and/or impoundment. In addition, Campus Recreation provides a free campus bike program, with a fleet of 100 cruiser bicycles and 20 mountain bikes available for student check out.